

AN ORDINANCE OF THE CITY OF REDMOND, WASHINGTON, AMENDING THE REDMOND MUNICIPAL CODE AND COMMUNITY DEVELOPMENT GUIDE BY REVISING THE CITY'S GOALS, POLICIES, PLANS AND REGULATIONS GOVERNING LAND USE AND DEVELOPMENT IN THE CITY CENTER NEIGHBORHOOD KNOWN AS THE CITY CENTER NEIGHBORHOOD PLAN, DGA-86-10.

WHEREAS, since adoption of the Redmond Community Development Guide in 1979, many events have occurred which have required a re-analysis of the City Center Neighborhood and the goals, policies, plans and regulations governing land use and development in this portion of the City, including:

1. Significant commercial growth;
2. Unprecedented increases in traffic;
3. Designation of Redmond as a Secondary Metropolitan Center within the context of the King County Subregional Plan; and
4. Adoption of Ordinance No. 1228, (Action Two) designating a site for a regional shopping center in the City Center design area No. 8.,

and

WHEREAS, the City Center Neighborhood and such goals, policies, plans and regulations have been re-examined through an extensive land use planning process which combined participation by City staff, members of the public and professional consultants, culminating in the following reports and recommendations:

Vision Workshop Report
Background Report
Alternatives Report
Draft and Final Environmental Impact Statement
Preliminary Recommendations Report,

and,

WHEREAS, the Redmond Planning Commission considered proposed revisions to the Community Development Guide related to land use and development in the City Center Neighborhood which were products of this land use planning process and which were expressed in these reports and recommendations by conducting seventeen public hearings and deliberating through the course of eleven public meeting study sessions, and

WHEREAS, after the conclusion of receipt of public testimony the Planning Commission formulated its recommendations on DGA-86-10 and such recommendations were transmitted to the City Council by Redmond Mayor Doreen Marchione on December 21, 1987, and

WHEREAS, the City Council received public testimony concerning the Planning Commission's recommendations at a public hearing and conducted a series of public meeting study sessions to consider such recommendations and public testimony, and

WHEREAS, based upon the foregoing process, the City Council has determined that the amendments to the Redmond Community Development Guide set forth hereinafter will promote the health, safety and welfare of the citizens of the City and surrounding communities and will provide for orderly growth and development of the City Center Neighborhood, now, therefore,

THE CITY COUNCIL OF THE CITY OF REDMOND, WASHINGTON, HEREBY ORDAINS AS FOLLOWS:

Section 1. Section 20B.60.030(45) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as follows:

60.030(45) Policy - Develop a pedestrian and bicycle street designation system to allow for the application of design standards which reflect the varying intensity of pedestrian and bicycle circulation. This system should provide for the following:

City Center

(a) Pedestrian Treatment

The Redmond Way/Cleveland Street Couplet should emphasize pedestrian circulation and the through movement of transit.

(b) Bikeway Treatments

Develop a City Center bikeway loop with connections to key interior locations. This central portion of the City's bikeway plan serves as an urban design feature providing a focal point for bicycling, and it can be accessed by area transportation corridors. The map entitled "City Center Bikeway Plans" is incorporated as part of this subsection.

- Overlake Business Area
- Residential Neighborhoods
- Business and Industrial parks

Section 2. Section 20B.60.030(65)(d) of the Redmond Municipal Code and Community Development Guide is hereby amended to read as follows:

20B.60.030(65)(d) - Provide for the development of a transit station in the City Center.

- . Transit ● tion development in the City C● er area should be provided on a staged schedule according to Metro service provisions, preserving adequate space to accommodate the long-term (completion) needs of a transit center.
- . The downtown transit station should conveniently serve City core businesses as well as the shopping center.
- . The City Center circulation plan should integrate a transit station with those access streets that will serve as the primary pedestrian corridors.
- . The vicinity leading to and surrounding a transit station should be well lighted and accessible to persons with special mobility needs.

Section 3. A new Section 208.60.030(125) is hereby added to the Redmond Municipal Code and Community Development Guide to read as follows:

208.60.030(125) Policy - Seek reversionary rights to the Burlington Northern Railroad right-of-way to be used for public purposes if the right-of-way is abandoned.

Section 4. A new Section 208.60.030(130) is hereby added to the Redmond Municipal Code and Community Development Guide to read as follows:

60.030(130) Policy - Discourage easement uses of the Burlington Northern Railroad right-of-way which preclude or obstruct potential future use for public purposes.

Section 5. Section 208.60.020(05) of the Redmond Municipal Code and Community Development Guide is hereby amended by revising the map entitled "Area Transportation Corridors" to read as set forth on the following map of the same title.

Section 6. Section 208.60.060(05) of the Redmond Municipal Code and Community Development Guide is hereby amended to read:

208.60.060(05) Arterial Street Plan - The Arterial Street Plan divides Redmond streets into four categories by function: freeways and expressways, principal arterials, minor arterials, and collectors. Local access streets are not included. By designating existing and proposed vehicular corridors and their functions, the Plan guides future projects that meet specific needs.

The maps entitled "Functional Classification and Street Plan", "City Center Arterial Street Plan" and the table entitled "Functional Arterial Classification Summary for Long Range Transportation Plan" are incorporated as a part of this subsection.

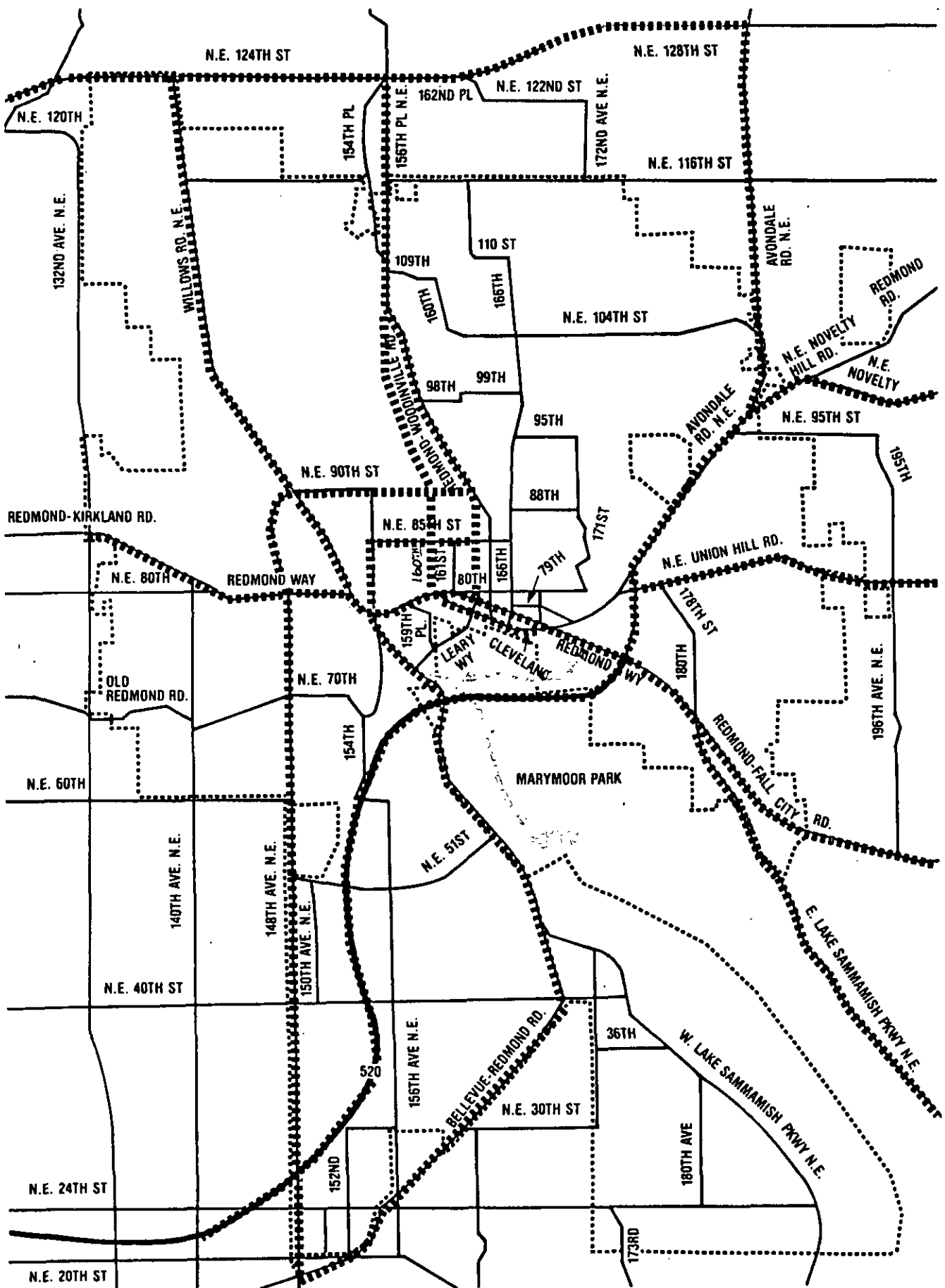
The following maps entitled "Functional Classification and Street Plan", "City Center Arterial Street Plan" and the table entitled "Functional Arterial Classification Summary for Long Range Transportation Plan" are hereby incorporated into said section.

Section 7. Section 20B.60.060(10) of the Redmond Municipal Code and Community Development Guide is hereby amended by incorporating the following map entitled "City Center Bike Plan" and such Section is amended to read as follows:

20B.60.060(10) Bikeway Plan - The Redmond Bikeway Plan provides for bikeways that can be used for recreation and transportation. Several types of bikeways can be provided, depending on their purpose and the conditions along their proposed route.

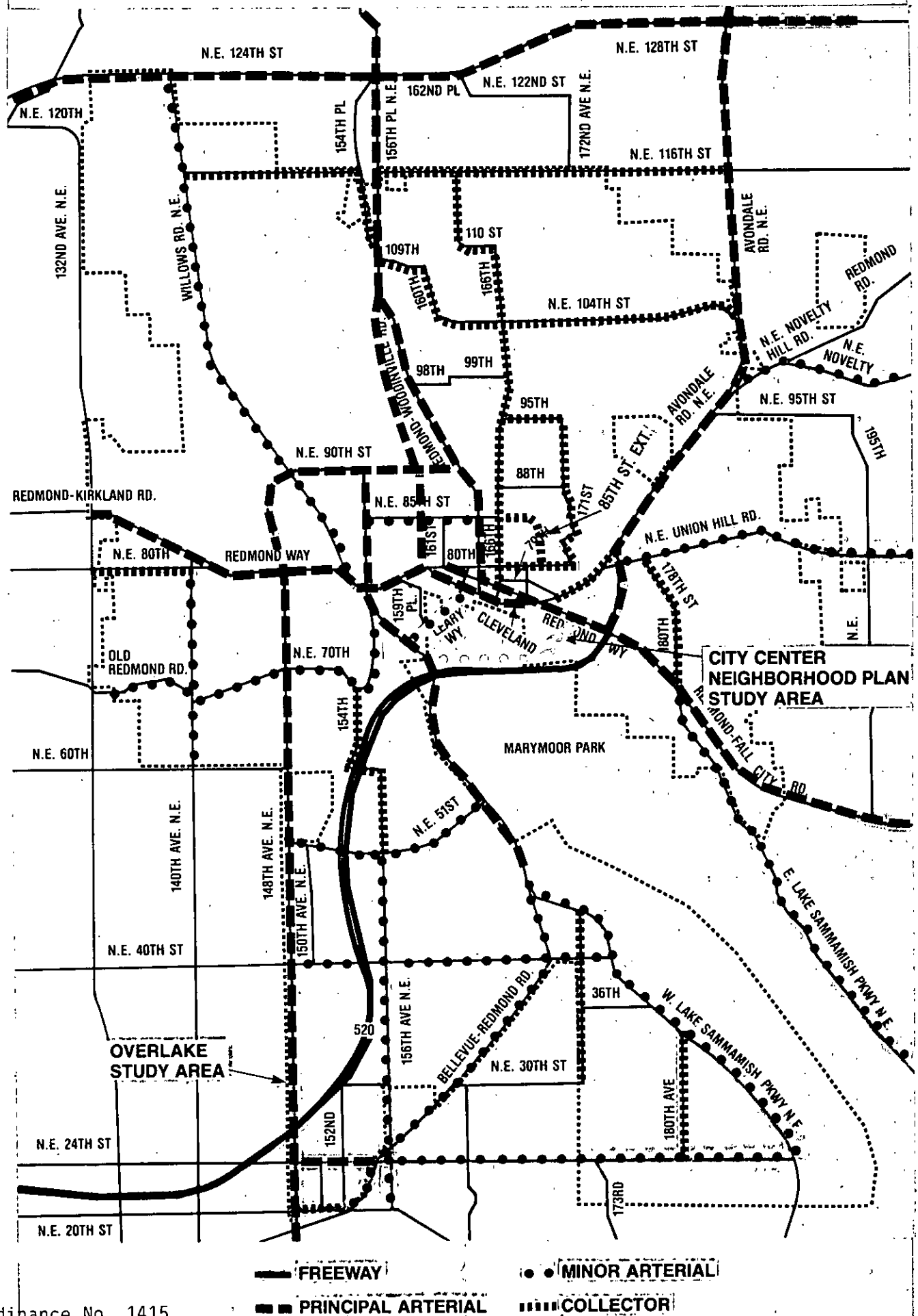
The maps entitled "Bikeway Plan" and "City Center Bike Plan" are incorporated as a part of this subsection.

AREA TRANSPORTATION CORRIDORS

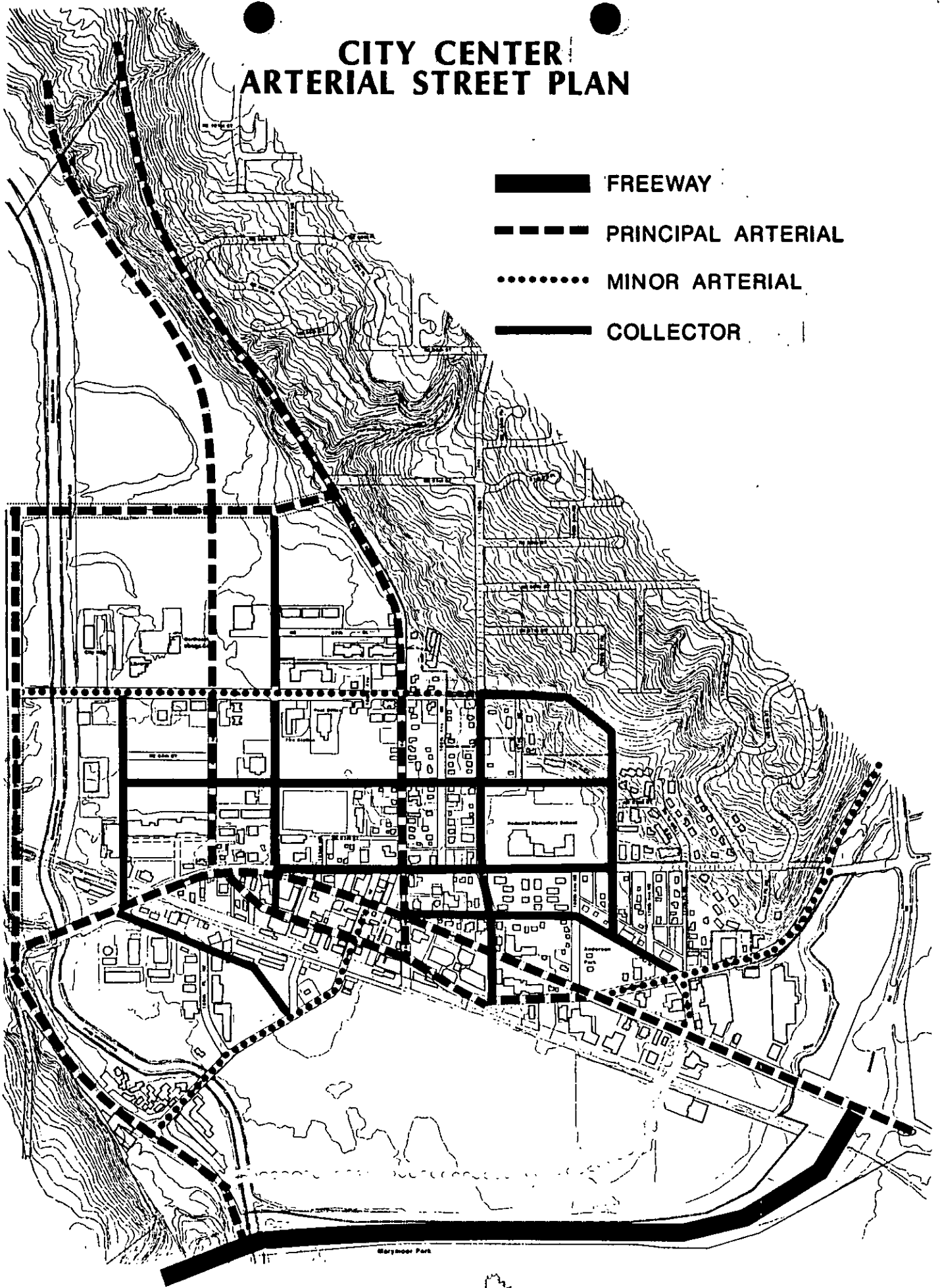






■■■■■ CORRIDORS

FUNCTIONAL CLASSIFICATION AND STREET PLAN



CITY CENTER ARTERIAL STREET PLAN

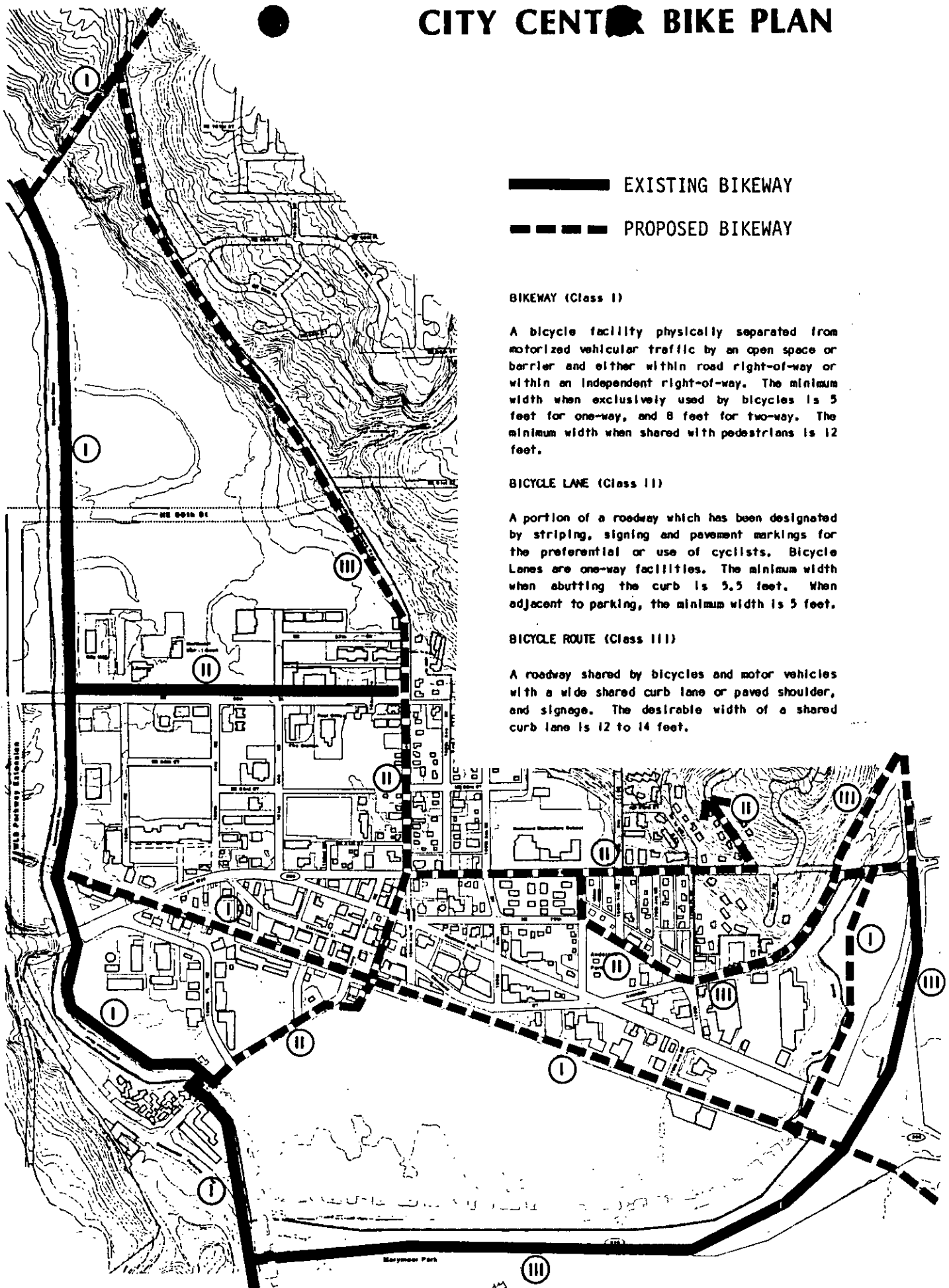


-  FREEWAY
-  PRINCIPAL ARTERIAL
-  MINOR ARTERIAL
-  COLLECTOR

Ordinance No. 1415



CITY CENTER BIKE PLAN



EXISTING BIKEWAY

PROPOSED BIKEWAY

BIKEWAY (Class I)

A bicycle facility physically separated from motorized vehicular traffic by an open space or barrier and either within road right-of-way or within an independent right-of-way. The minimum width when exclusively used by bicycles is 5 feet for one-way, and 8 feet for two-way. The minimum width when shared with pedestrians is 12 feet.

BICYCLE LANE (Class II)

A portion of a roadway which has been designated by striping, signing and pavement markings for the preferential or use of cyclists. Bicycle Lanes are one-way facilities. The minimum width when abutting the curb is 5.5 feet. When adjacent to parking, the minimum width is 5 feet.

BICYCLE ROUTE (Class III)

A roadway shared by bicycles and motor vehicles with a wide shared curb lane or paved shoulder, and signage. The desirable width of a shared curb lane is 12 to 14 feet.

Ordinance No. 1415

0 500 1000 feet



City Of Redmond, Washington
Department Of Planning and Community Development

**PROPOSED FUNCTIONAL ARTERIAL CLASSIFICATION SUMMARY
FOR LONG RANGE TRANSPORTATION PLAN**

STREET	TRAFFIC LANES*		SIDEWALKS		BIKE LANES**	
	EXISTING	PLANNED	EXISTING	PLANNED	EXISTING	PLANNED
PRINCIPAL ARTERIALS:						
STATE ROUTE 520	2-4	4	0	0	2	2
REDMOND WAY (except couplet)	4/5	4/5	0-2	2	0	0
REDMOND WAY/CLEVELAND STREET COUPLET	3	2-3	2	2	0	0
148TH AVENUE N.E.	4	4	2	2	0	0
RED-WOOD ROAD (164TH AVE. N.E.)	2-4	2	0-2	0-2	0	0-2
AVONDALE ROAD	2	4	0	2	1	2
148TH AVE. N.E. EXTENSION	0	4	0	2	0	0
SR 901 EXTENSION/HLSP EXTENSION)	0	4/5	0	0	0	0
N.E. 24TH STREET (148TH TO 156TH)	4	4	2	2	0	0
W. LK. SAMMAMISH PKWY (BEL- RED ROAD TO REDMOND WAY)	2	4	0	2	0-2	2
N.E. 124TH STREET	2	2-4	0	0-2	0	2
160TH AVE. N.E.	2	2-4	0	2	0	0
N.E. 90 TH ST. (WILLOWS TO SR 202)	0	4-5	0	2	0	0
MINOR ARTERIALS:						
LEARY WAY	2	3	0-2	1-2	0	2
N.E. 85TH STREET	4	4	1-2	2	2	2
N.E. 24TH STREET (156TH TO W. LK. SAMMAMISH PKWY.)	2	2	2	2	1	2
W. LK. SAMMAMISH PKWY 156TH AVENUE N.E. (N.E. 20TH to N.E. 51ST)	2	2	0	0	1	1-2
156TH AVENUE N.E. (N.E. 20TH to N.E. 51ST)	4	4	2	2	2	2
N.E. 51ST STREET	2-4	2-4	1-2	2	0	0-2
N.E. 40TH STREET	2-4	2-4	0-2	2	0-2	2
N.E. OLD REDMOND ROAD	2	2-4	0-2	2	0-2	1
140TH AVENUE N.E.	2	2	0	2	0-1	1
E. LK. SAMMAMISH PKWY	2	4	0	0	0	2
N.E. UNION HILL ROAD	2-4	2-4	0-2	0-2	0	0-2
NOVELTY HILL ROAD	2	2	0	0	0	1-2
BEL-RED ROAD	2-4	4	0-2	2	0	2
WILLOWS ROAD	2	2	0-2	1-2	1-2	2
132ND N.E.	2	2	0-2	1-2	0	1-2
COLLECTORS:						
158TH AVE. N.E. (N.E. 86TH ST. TO REDMOND WAY)	0-2	4/5	0-1	2	0	0
159TH AVE. N.E.	2	2	0	0-2	0	0
156TH AVE. N.E. (N.E. 51ST TO OLD RED ROAD)	2	2	0-2	2	0	1
161ST AVE. N.E. (N.E. 90TH ST. TO CLEVEL- LAND ST.)	0-4	4/5	0-1	2	0	0
N.E. 116TH STREET	2	2	0	1	0	2
N.E. 104TH STREET	2	2	2	2	1	1
N.E. 100TH	2	2	2	2	0	2
166TH AVE. N.E.	2-4	2-4	2	2	0-2	0-2
N.E. 95TH STREET	2	2	1	2	0	1
N.E. 80TH STREET (REDMOND WAY TO 171ST AVE.)	2	2-4	0-2	2	0	2
172ND AVE. N.E.	2	2	2	2	1	1
N.E. 20TH STREET	4	4	0-1	2	0	0
180TH AVENUE N.E. (SOUTH OF UNION HILL ROAD)	2	2	0	2	2	2
N.E. 80TH ST. (132ND AVE. N.E. TO 140TH AVE.)	2	2	1	2	1	1
AVONDALE WAY	4	4	2	2	0	0
171ST AVE. N.E.	2	2	1-2	1-2	1	1
N.E. 85TH STREET EXTENSION	0-2	2	1	2	0	0

* May include left turn lanes or two-way left-turn lanes except as noted by 2/3 or 4/5.

**Does not include Class I or Class III Bicycle facilities

Section 8. Section 20B.85.130 of the Redmond Municipal Code and Community Development Guide is hereby amended to read:

20B.85.130 CITY CENTER

The City Center is roughly bounded by Education Hill and Bear Creek on the north, Bear Creek on the south and east, and the Sammamish River on the west. The intent of the City Center Plan is to blend Redmond's natural setting with economic activities to create a fiscally healthy, unique and attractive downtown area. It is important that the City Center be the focus of the community and as such the Plan be a statement about the community's aesthetic, functional and cultural aspirations.

FUNCTION: The long term economic vitality of the City Center is critical to the aesthetic as well as functional objectives of Redmond. It must not only be an employment center, but must be a lively and safe place to be. The plan provides for a variety of uses, and clusters them in complementary groupings. This is intended to consolidate and promote a higher density and concentrated retail core with increased activity. This concentrated retail grouping is designed to create a focus to the City Center.

DESIGN AND AESTHETICS: It is the intent of the plan to retain key natural features, and large open spaces such as the Sammamish River, Bear Creek and park areas. This would be achieved through a landscaping program based on the concept of the downtown as an urban park designed to support a commercial and employment center of regional scale. This concept will be implemented by "soft" or informal landscaped areas on the edges of the City Center which act as transitions to the adjacent neighborhoods and Marymoor Park.

Walkways located throughout the downtown will be designed to connect to these edges and to link both formal and informal gathering places within the commercial area. These edges should be designed to provide a sense of entry to this area. Walkways and other landscaped areas should

emphasize the use of native plant materials to help retain the sense of this area as a valley floor when viewed from the adjacent hillsides.

Consistent with this image the bicycle theme should be strengthened and expanded on. Bicyclists will be encouraged to enter into the downtown area from the Sammamish River Trail.

85.130(05) City Center Goal - Utilize Redmond's history, natural setting and growth opportunities to develop a unique, attractive and economically healthy City Center offering a variety of service, office, retail shopping, living and recreational opportunities.

(a) Policy - The City Center is designated as the activity center and classified as a secondary metropolitan center for the purpose of implementing the King County Subregional Plan in establishing priority areas for the concentration of employment and commercial growth and the allocation of transportation funding.

(b) Discourage expansion of City Center boundaries.

(c) Encourage City Center as the focus of retail, office and service uses for the Redmond Planning Area.

85.130(10) City Center Goal - Promote the development of the City Center as the primary commercial activity center and destination location for the City and greater Redmond area.

(a) Policy - Provide a location for a variety of office, service, retail shopping and living opportunities.

(b) Policy - Encourage the development of a major comparison center area in downtown Redmond which would expand business opportunities, provide additional revenue and be a positive influence in the development of the City.

(c) Policy - Require comparison shopping center proposals to integrate the shopping facilities visually, functionally, and through pedestrian connections into the City Center.

(d) Policy - Encourage and promote the City Center as a location for both large and small business which are compatible with the long term economic health of the community.

(e) Policy - Encourage clustering of associated business uses to reduce conflict among uses and to increase convenience for businesses and users by:

- . Encourage comparison retail and associated uses at the center of the downtown area to encourage development of a concentrated shopping area which acts as an active, people-oriented focus to the downtown and City.
- . Encourage pedestrian retail and associated uses at the center of the downtown area to encourage development of a concentrated shopping area which acts as an active, healthy downtown and city.
- . Encourage office uses located adjacent to the retail core.

(f) Policy - Encourage public and private sector cooperation in providing capital investment such as parking, and street improvements which contribute to retaining existing businesses and encouraging new business to locate in the City Center.

(g) Policy - Encourage the development of housing in and adjacent to the City Center to support business activities and increase the vitality of the area.

(h) Policy - Encourage commuter traffic to use bypass opportunities which will reduce traffic on City Center streets and allow easy access to businesses.

(i) Policy - Encourage City Center employees to use transit, car pools and other forms of transportation which reduce congestion and free up space for customer parking.

(j) Policy - Encourage shared, clustered parking to reduce the total number of overall stalls needed and increase the economic and aesthetic potential of the area.

85.130(15) City Center Goal - Encourage public and private development to retain and enhance the natural features in the City Center area to create a unique and attractive commercial park setting which will establish the image and

character for development throughout the City.

(a) Policy - Encourage landscaping plans which are informal, retain desirable trees and use native plant materials.

(b) Policy - Encourage informally designed landscaping and open space on the edges of City Center to act as a transition to adjacent neighborhoods and Marymoor Park.

(c) Policy - Special architectural and landscaping features should be developed at all major entrances to the City Center.

(d) Policy - Development adjacent to the Sammamish River, Bear Creek and other park areas should be designed to complement and enhance these areas by such techniques as:

- . Fronting buildings and associated entrances on the park.
- . Providing convenient public access to the park.
- . Complementing the park with additional landscaping, plazas and other pedestrian features.
- . Avoid locating parking lots, auto-oriented signing and service areas toward the park space.

(e) Policy - Use landscaping and other forms of screening to soften the appearance of parking lots, backs of buildings and service areas from public streets, walkways and adjacent hillsides.

(f) Policy - Existing parks in the City Center Neighborhood should be retained and enhanced.

Opportunities to acquire additional parks and open space shall be consistent with Chapter 20B.50 PARKS, RECREATION AND OPEN SPACE of the Community Development Guide.

85.130(20) City Center Goal - Encourage the development of the City Center as the cultural focus of the greater Redmond area.

(a) Policy - Encourage the development of a performing arts center in the City Center.

(b) Policy - Encourage the development of public art features with all development.

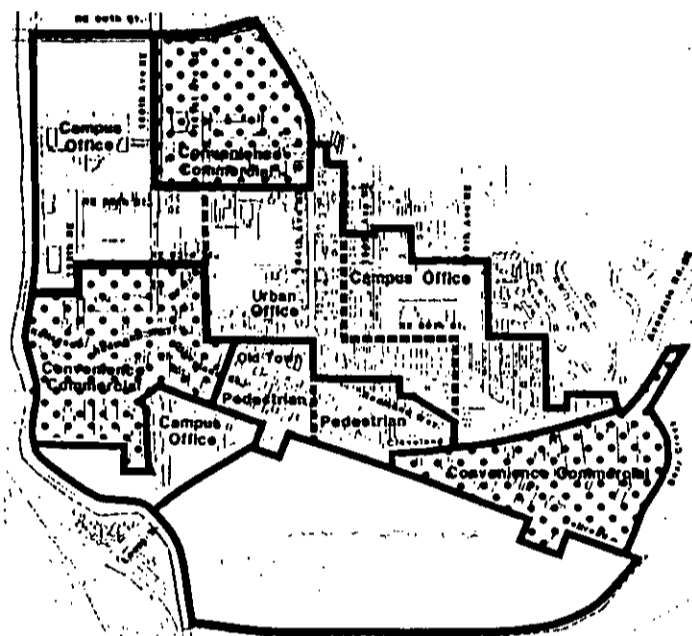
(c) Policy - Create a program which locates public art

features in key locations in the City Center.

85.130(25) City Center Goal - Foster Redmond's image as Bicycle Capital of the Northwest.

(a) Policy - Provide bicycle facilities such as connections to the Sammamish River Trail, bicycle racks in conjunction with new developments, bike lanes on key streets and signage at key points.

(b) Policy - Encourage bicycle races and other cycling related activities in the City Center.



85.130(30) Convenience Commercial Design Area Goal -

Provide low scale centers for convenience retail business as well as other compatible commercial, service and office uses that require convenient vehicular access and significant parking.

(a) Policy - Encourage business driveway access on local streets rather than the arterials wherever feasible.

(b) Policy - Encourage joint use of driveways and parking to minimize vehicle turning conflicts and reduce overall parking needs.

(c) Policy - Separate and/or buffer walkways from vehicular circulation areas.

(d) Policy - Avoid the creation of lots which increase the number of driveways on Redmond Way, Cleveland Street, Avondale Road and the Redmond-Woodinville Road.

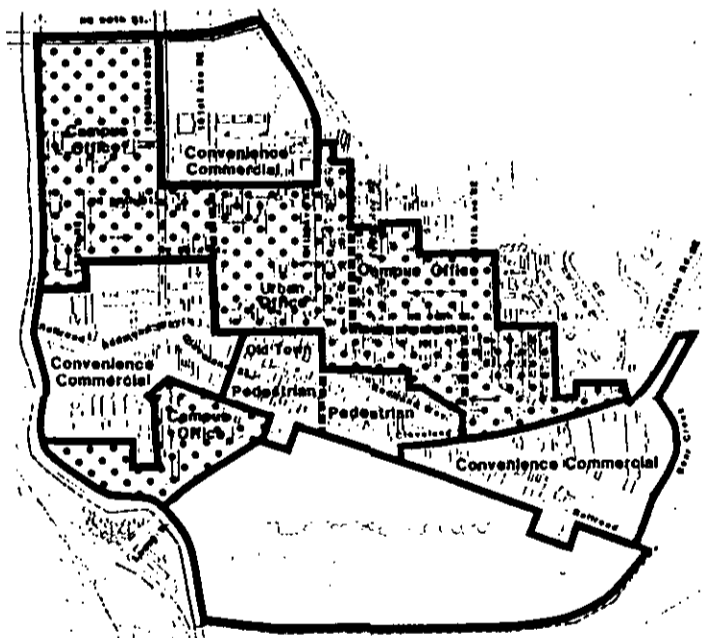
85.130(35) Convenience Commercial Design Area Goal -

Provide for low scale commercial areas which are attractively designed and reflect the importance of their position as entrances to the City Center.

(a) Policy - Avoid the orientation of service areas, the back of buildings and unscreened parking toward parks, walkways, arterials and freeway areas.

(b) Policy - Recognize the importance and a potential to create a strong visual image for the neighborhood by improving the appearance of Redmond Way and the Redmond-Woodinville Road at the entrances to the City Center.

(c) Policy - Work with Burlington Northern to enhance the appearance of the railroad bridge over Redmond Way to act as an entrance feature.



85.130(40) Office Design Area Goal - Encourage development of office uses that provide transition from retail and residential uses, and stimulate concentrated economic activity and employment in the City Center.

(a) Policy - Encourage professional, financial and commercial office land uses, with associated uses such as business and personal services.

(b) Policy - Encourage a mix of office, residential and limited retail within the district and within structures.

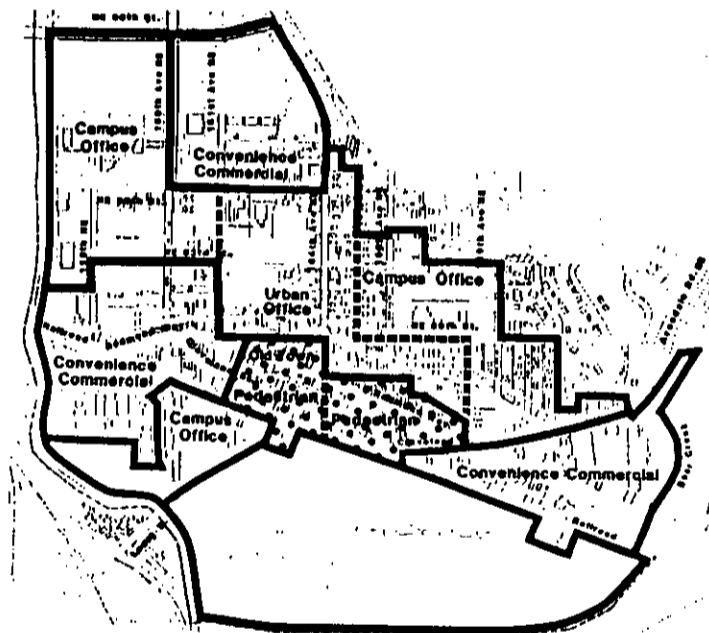
(c) Policy - Encourage higher density of office-type structures adjacent to the pedestrian-oriented core to support and focus activity in pedestrian areas.

(d) Policy - Provide a balance of vehicle and pedestrian circulation by encouraging structured parking conveniently connected to the pedestrian linkage system and central core area.

(e) Policy - Provide adequate open space and pervious surface to mitigate the impacts of higher density structures.

(f) Policy - Encourage retail use at ground level of office buildings near pedestrian areas.

(g) Policy - Develop a system of public and private open spaces that link activities at the street level.



85.130(45) Pedestrian Design Area Goal - Provide an area for pedestrian-oriented retail activities which visually and functionally complements and benefits from the retail shopping in the Old Town area and proposed Regional Shopping Center.

(a) Policy - Provide a visually significant and direct connection to the adjacent shopping center and Old Town to allow these areas to function as a unified retail area.

(b) Policy - Provide for pedestrian amenities such as benches, kiosks, plazas, public art and wide sidewalks.

(c) Policy - Encourage window and merchandise displays next to pedestrian walkways.

(d) Policy - Encourage the modulation of buildings facing the walkways to provide areas for outdoor restaurants, displays and gathering areas.

(e) Policy - Work with Burlington Northern Railroad to develop a landscaping and pedestrian treatment for the railroad right-of-way that allows easy access along and across it and reduces its visual impact on the area.

(f) Policy - Encourage the development of a parking program for this area that:

- . Minimizes on-site parking to allow development of a compact shopping district.
- . Provides for on-street parking.
- . Explores the potential of providing additional surface and/or structured parking adjacent to this design district.
- . Encourages development and signing adjacent to the Old Town area to complement rather than dominate the design objectives of that area.

85.130(50) Pedestrian Design Area (Old Town) Goal -

Maintain and enhance pedestrian activities in Old Town to emphasize the human scale of the area.

(a) Policy - Ensure that public improvements in the area (streets, walkways, lighting and landscaping) complement the pedestrian activity in Old Town.

(b) Policy - Encourage the installation of benches, kiosks and other street furniture which provide a unifying element and aid in developing the historical character and pedestrian scale of the area.

(c) Policy - Encourage the use of financial mechanisms available to fund the installation of pedestrian-related sidewalk improvements (public, private or combination).

85.130(55) Pedestrian Design Area (Old Town) Goal -

Encourage and promote the pedestrian use of outdoor spaces in Old Town.

(a) Policy - Encourage the use of parks or "people places" in or along the streetscape.

(b) Policy - Create a visual pedestrian feature, e.g., fountain, square, sculpture or other focal point that will draw people to parks or "people places" along the streetscape.

(c) Policy - Encourage strong pedestrian linkages with other existing and potential commercial development in the City Center.

85.130(60) Pedestrian Design Area (Old Town) Goal - Maintain and encourage an attractive and high quality environment for Old Town through specific attention to architectural detail, components of the streetscape and the relationships between them.

(a) Policy - Maintain height limitations in Old Town that are appropriate to the pedestrian environment.

(b) Policy - Encourage new infill development in Old Town which is compatible to the historic character of the area.

85.130(65) Pedestrian Design Area (Old Town) Goal - Adopt criteria and standards for Old Town which encourage a balance between the need to realize economic viability and the achievement of development complementary to the historical character and function of Old Town.

(a) Policy - Create a design district overlay zone in Old Town to evaluate new development.

(b) Policy - Develop an Old Town Design Plan to address specific street treatments for gateway areas.

(c) Policy - Develop a sign program which reflects the Old Town character of the area and pedestrian nature of the district.

85.130(70) Pedestrian Design Area (Old Town) Goal - Emphasize the natural and manmade features of Old Town and the surrounding area which achieve a balanced, pleasant urban space.

(a) Policy - Encourage Redmond's historic character of architectural styles predominant during the late 19th century and early 20th century.

(b) Policy - Encourage an intensified planting program in Old Town through the use of special median planting, flower baskets and/or other seasonal plant displays.

85.130(75) Pedestrian Design Area (Old Town) Goal - Create opportunities for economic development that will promote a healthy business environment in Old Town.

(a) Policy - Actively support economic development measures that serve to retain and promote the growth of business and attract new businesses that are compatible with the scale of Old Town.

(b) Policy - Encourage pedestrian-oriented retail businesses to locate in Old Town.

(c) Policy - Encourage mixed use development possibilities which incorporate retail uses on ground floor with service or housing on upper stories.

(d) Policy - Encourage economic incentive programs that will encourage historic redevelopment of structures and provide new business opportunities.

(e) Policy - Encourage a variety of economic activities that will provide for pedestrian uses in Old Town during the day as well as the evening.

85.130(80) Pedestrian Design Area (Old Town) Goal - Encourage the development and redevelopment of the area in a manner which maintains a character and scale consistent with its role as the historic Old Town center and focal point of the community.

(a) Policy - Encourage the preservation and restoration of structurally sound historic buildings in a manner which is consistent with the Old Town character of the area.

(b) Policy - Encourage public and private sector partnerships for the historic redevelopment of buildings.

(c) Policy - Allow flexibility in zoning and building codes to encourage the preservation and rehabilitation of buildings of historic value.

85.130(85) Pedestrian Design Area (Old Town) Goal - Encourage the balance of function between the need for

through-traffic opportunities as well as the re-definition of Old Town as a traffic destination.

(a) Policy -- Avoid 4-lane arterials through Old Town.

(b) Policy -- Provide on-street parking.

(c) Policy -- Integrate public transit, bus stops and information kiosks with streetscape.

Section 9. Section 20C.10.200 of the Redmond Municipal Code and Community Development Guide is hereby repealed and Sections 20C.10.210 "Business Park District (BP)", 20C.10.220 "Light Industry District (LI)" and 20C.10.230 "Heavy Industry District (DI)", are each renumbered to Sections 20C.10.200, 20C.10.210 and 20C.10.220, respectively.

Section 10. The City's Official zoning Map is hereby amended as set forth herein and a new Section 20C.10.230 is hereby added to the Redmond Municipal Code and Community Development Guide to read as follows:

20C.10.230(05) CITY CENTER DESIGN AREAS - The City Center is divided into four Design Areas entitled, "Convenience Commercial", "Office", "Pedestrian", and "Mixed Use/Shopping Center". These are delineated by the map entitled "City Center Design Areas", included in this section.

20C.10.230(10) CITY CENTER LAND USE AND SITE REQUIREMENTS - The chart entitled, "City Center Land Use and Site Requirements" is incorporated as a part of the subsection. It indicates what land uses are permitted in the Design Areas and other restrictions related to these uses. The intent of each of the Design Areas is as follows:

- (a) Convenience Commercial - This Design Area for retail merchandise and services of daily consumption such as entertainment, food, dry cleaners, florists and similar uses. These areas are located near entrances to the City Center and are oriented primarily to automobile access. Residential uses are also encouraged.
- (b) Office - The Office Design Area is established to provide for uses such as financial and professional services, public administrative offices, health services, residential uses and other activities normally conducted in higher density multi-story structures. The Office Design Area is separated into two sub-classifications, Urban and Campus, which provide for a differentiation in intensity of use, particularly building height, near the center of the City Center District.

- (c) Pedestrian - The Pedestrian Design Area is an area of mixed retail, service and residential uses which provide a focus of pedestrian activity between the shopping center and the office areas. It incorporates the historical area (Old Town) and links the shopping center to other parts of the City Center. The intent of this Design Area is to limit automobile use to accommodate pedestrian activity and encourage recreational and cultural uses.
- (d) Mixed Use/Shopping Center - The intent of this Design Area is to provide for a major regional shopping center facility and associated uses such as offices, other retail activities, residential, theaters and a hotel. The Design Area is designed to focus activity adjacent to the existing center of the City Center District and provide for pedestrian and automobile integration between the two areas.

20C.10.230(15) - For purposes of administering the "City Center Land Use and Site Requirements" chart, the following definitions are noted:

- (a) Services: Services are professional, commercial and public activities including lodging, personal services, business and financial services, minor repair services, entertainment and recreation services, health services, social services, legal services, educational services and cultural services. Services such as industrial laundries and dry cleaning, automotive repair services, testing laboratories, publishing, warehousing/storage and similar uses are considered industrial uses and are excluded from this definition.
- (b) Retail Uses: Retail uses are establishments engaged in selling merchandise to the general public for personal or household consumption; processing of products does not occur or is only a minor part of use (restaurants, butcher shops, etc.). General

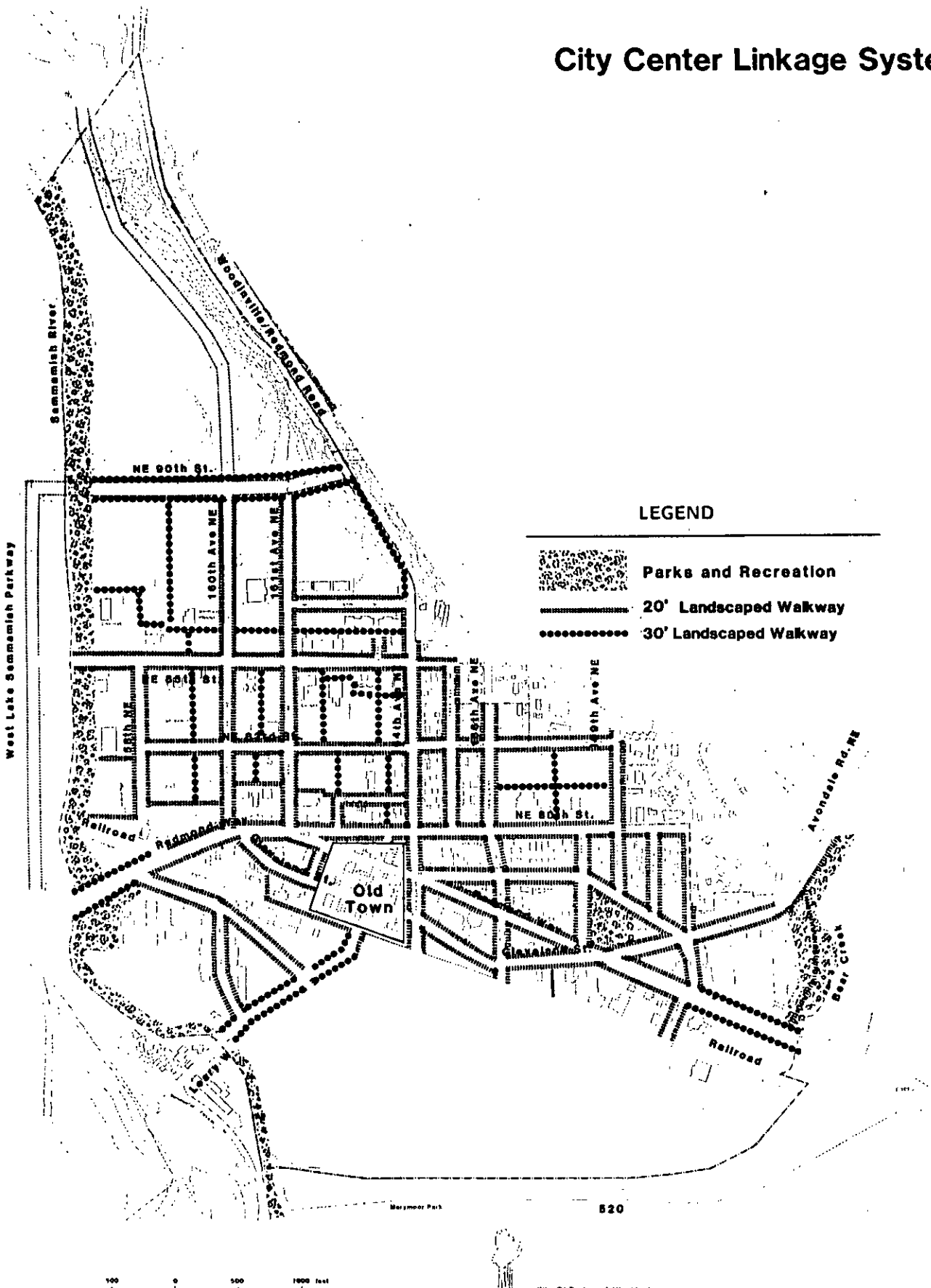
categories include food stores, service stations, apparel, furniture and home furnishings, eating and drinking places, and general merchandise.

- (c) Public Facilities and Utilities: Public facilities include governmental administrative offices, libraries, parks, police and fire stations, educational institutions, cultural facilities, community or senior centers, parking structures, local utility supply lines or substations. Utilities are gas, water, sewer, electricity, telephone and cable television.

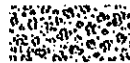


20C.10.230(20) SPECIAL DEVELOPMENT CRITERIA - BUILDINGS

OVER FOUR STORIES - The standards set forth in 20C.20.235(70) are defined to guide review of Special Development Permits for buildings exceeding the four story height limit in the Urban Office Area. These requirements shall be met prior to issuance of a Special Development Permit for buildings exceeding four stories.

City Center Linkage System



LEGEND

-  Parks and Recreation
-  20' Landscaped Walkway
-  30' Landscaped Walkway

20C.10.230(10) CITY CENTER LAND USE AND SITE REQUIREMENTS

LAND USES	Design Areas				
	Convenience Commercial	Office		Pedestrian	Mixed Use Center ^⑨
		Urban	Campus		
Retail ^⑥	P	P ^①	P ^②	P	P
Services (Commercial, Professional, Public) ^⑥	P	P	P	P	P
Residential:					
Multi-Family/Sr. Housing	30 du/ac	30 du/ac	30 du/ac	30 du/ac	30 du/ac
Sr. Housing with Bonus ^⑦	SDP	SDP	SDP	SDP	SDP
Public Facilities and Utilities	P	P	P	P	P
SITE REQUIREMENTS					
Minimum Open space (Includes Linkage System, Internal Landscaping, Amenities)	25%	30%	25%	20%	⑧
Maximum Height ^⑩	2 stories 30 feet	4 stories ^③ 50 feet	4 stories 50 feet	3 stories 40	3 - 8 stories ^⑤
Drive-through Facilities	P		④ P		

- 1 No single-story retail permitted. Retail permitted on ground floor only, except restaurants, delicatessans and taverns, which are allowed on any floor. Retail required on ground floor in buildings over 4 stories.
- 2 No single-story retail permitted. Restaurants, taverns and delicatessans are allowed on any floor. Other limited retail and services such as a copy shop, barber shop, magazine and snack shop, flower shop, etc. may be allowed on the ground floor if the structure is four stories in height. The intent of the other limited retail use is to serve office workers in the immediate area of the use.
- 3 Buildings over 4 stories permitted with SDP. Governed by Section 20C.20.235(70)(f).
- 4 Multi-story buildings only.

- 6 Adult entertainment is governed by Section 20C.20.015 of the Community Development Guide.
- 7 Governed by Section 20C.20.235(70)(c) Senior Citizen Housing Development - Density Bonus.

10 Both the stories and height standard must not be exceeded.

20C.10.230(25) CITY CENTER LINKAGE SYSTEM - The City Center Linkage System is a network of landscaped pedestrian ways and small greenbelts. It is established to provide safe pedestrian routes, enhance the appearance of buildings and their settings, provide a unified design element to offset varying architectural styles, and to soften the appearance of parking lots and service storage areas. Planting is intended to provide street trees and other vegetation appropriate for an urban setting. Massing and aggregating of plantings to achieve a strong, positive statement is encouraged. Use of seasonal color and ease of maintenance are plant characteristics that should also be considered.

(a) Installation of Linkage System - The various components of the Linkage System shall be provided as noted on the map entitled, "City Center Linkage System" which is incorporated as a part of this section. As property is developed or redeveloped, corresponding portions of the system shall be installed or otherwise provided for by the property owner/developer.

(b) Linkage System Description

Type I - A 30-foot landscaped walkway which includes an 8-foot sidewalk and 22 feet of planting. Type I is most typically used for internal block linkages and entry arterials to City Center. Average width is 30 feet with a minimum width of 15 feet.

Type II - A 20-foot landscaped walkway which includes an 8-foot sidewalk and 12 feet of planting.

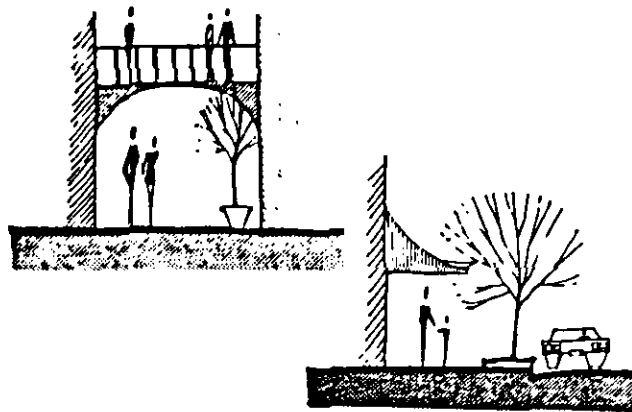
EXCEPTION - There shall be a 20-foot landscaped walkway which includes a 10-foot sidewalk and 10 feet of planting on 164th Avenue N.E. south of N.E. 85th Street and on the Redmond Way/Cleveland Street

Couplet. Type II is most typically used for streets and some internal block linkages. Average width is 20 feet with a minimum width of 10 feet.

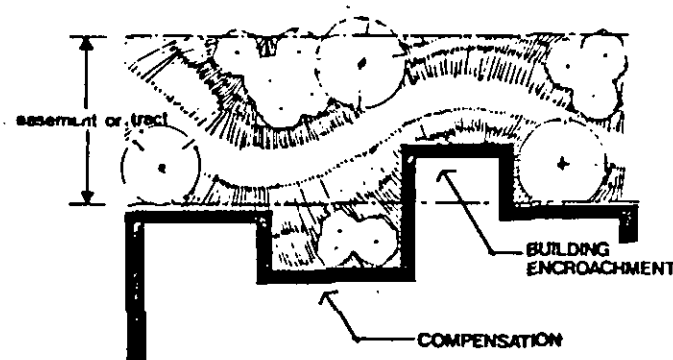
(c) Easements - Where a Linkage System walkway exists or is required outside of a public right-of-way, an easement for the City of Redmond may be required to provide continuity of the walkway to adjoining property.

(d) Permitted Encroachments - Buildings may extend into or over required linkage system landscaping or walkways when:

- . The encroachment is integrated into the linkage system by providing a covered walkway plaza or it otherwise complements pedestrian activities;
- . The required average and minimum width of the linkage system is maintained; a ten-foot building setback is maintained from interior block property lines; except that no setback is required when adjoining properties are under common ownership or a written agreement is obtained from the adjacent property owner.



Buildings, marquees and roof projections may extend over linkage systems when they enhance pedestrian activities.

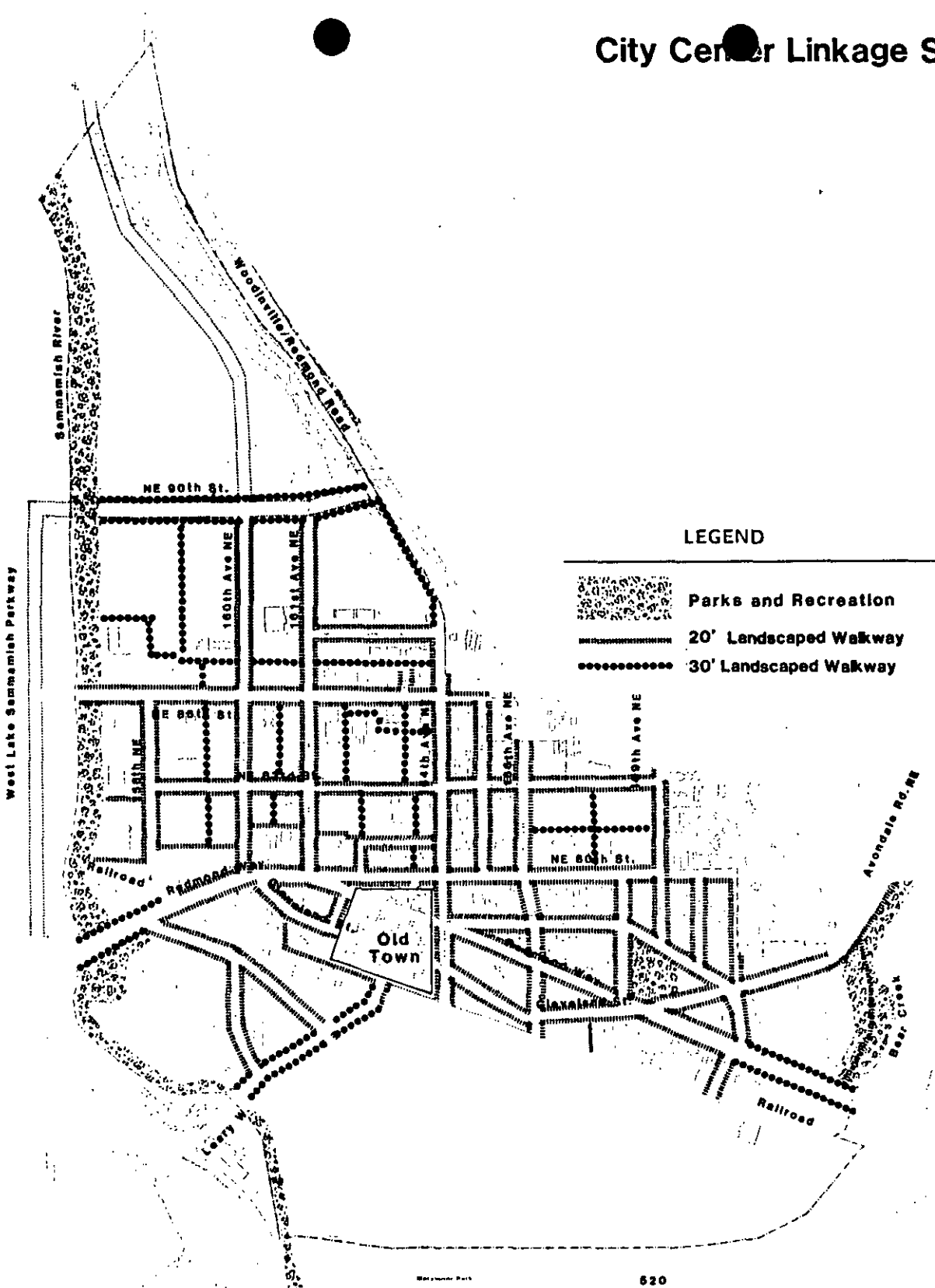


Buildings may encroach into a linkage system when compensation is provided at another location.




- (e) Width Measured from Curb - Where a Linkage System adjoins a public street, the system's width shall be measured from the edge of the existing or proposed curb.
- (f) Construction Standards - Construction standards for sidewalks are identified in Appendix H, CITY CENTER LINKAGE SYSTEM CONSTRUCTION SPECIFICATIONS.
- (g) Driveway Crossings - Driveways crossing the Linkage System should be minimized and joint use of driveways encouraged to separate vehicles and pedestrians. Areas in driveways will not be calculated as part of the area required to be landscaped in the Linkage System.
- (h) Access to Buildings - Pedestrian access from primary building to Linkage System shall not be interrupted by vehicular circulation, parking or other elements which discourage pedestrian use.
- (i) Interior Block Linkage System - Interruptions of mid-block linkage systems by vehicular circulation or parking shall be minimized.
- (j) Variations Exceeding Standards - Variations in the linkage system which exceed the standards may be approved by the Technical Committee. The variation, in the judgment of the Technical Committee, must not create a linkage system out of character and harmony with the surrounding linkage systems.
- (k) Variations not meeting standards - Variations in the linkage system which do not meet minimum standards may be approved by the Technical Committee. Variations may be allowed after consideration of factors, including, but not limited to:
- . existing right-of-way available to meet standards
 - . existing buildings encroaching in linkage area
 - . pedestrian and vehicular volumes anticipated
 - . existing vegetation
 - . disruption of system continuity
 - . accessibility to buildings

- (l) Old Town - The area identified on City Center Linkage System Map as "Old Town" requires the sidewalk component only.
- (m) Walkway Separation - On heavily traveled roadways without on-street parking or bike lanes to protect pedestrians, planting areas should be used to separate the walkways from the curb.

City Center Linkage System



LEGEND

-  Parks and Recreation
-  20' Landscaped Walkway
-  30' Landscaped Walkway

City Of Redmond Washington
 Department Of Planning and Community Development

Section 11. A new Section 20C.20.235(70)(f) is hereby added to the Redmond Municipal code and Community Development Guide to read as follows:

(f) Buildings Over Four Stories - Buildings may rise over four stories in the City Center District, Urban Office Design Area provided that:

- A. Structured or underground parking is required.
- B. A Transportation Management Plan is required.
- D. An increase must be accompanied by retail on the ground floor. A small portion of the ground floor can be used for non-retail uses in areas determined by the Technical Committee to have very low potential for suitability for retail leasing. In no circumstances shall the non-retail component exceed 25% of the leasable area. Retail on the ground floor shall be oriented to the linkage system whenever possible.
- E. Maximum building heights shall be as follows:
 - 1. 6 stories (75 feet) for office use with ground floor retail.
 - 2. 8 stories (100 feet) if at least 50% of floor area is in residential use.
- F. Public plazas or other public facilities at ground floor are required to provide connections with the linkage system.
- G. Architectural character must be harmonious with the cityscape.
- H. Environmental impacts must be satisfactorily mitigated in a manner consistent with the design objectives of the district.

Section 12. Section 20C.20.150(10) of the Redmond Municipal Code and Community Development Guide is hereby amended by revising the table entitled "Parking Spaces Required" incorporated therein to read as set forth on the following table with the same title.

Section 13. Section 20C.20.150(20) of the Redmond Municipal Code and Community Development Guide is hereby amended by repealing subsection (a) thereof entitled "Downtown Parking District - Waiver of Requirements" and re-lettering the remaining subsections (b) - (f) to (a) - (e) correspondingly.

Section 14. Section 20C.20.140(10) of the Redmond Municipal Code and Community Development Guide is hereby amended by revising the table entitled "Requirements for Outdoor Storage" incorporated into said section to read as set forth on the following table with the same title.

Section 15. Section 20C.20.230(30) of the Redmond Municipal Code and Community Development Guide is hereby amended by revising the table entitled "Sign Requirements Per Zoning District" to read as set forth on the following table with the same title.

Amend Section 20C.20.150 Parking as follows:

PARKING SPACES REQUIRED			
ZONING DISTRICT		REQUIRED OFF-STREET PARKING	
		Minimum Required	Maximum Allowed
General	G	2/du	NS
Agriculture	A		
Ranch Estates	RE		
Residential Estates	RI		
Suburban Estates	R2-R3		
Suburban Residence	R4-R5-R6		
Urban Residence	RB-R12	1.2/Studio du	NS
Multiple Residence	R20-R30	1.5/1 Bedroom du# 1.8/2 Bedroom du# 2.0/3+ Bedroom du#	
Professional Office	PO	3.5/1000 sq.ft.gfa	4.5/1000 sq.ft.gfa
Neighborhood Business	NB	4/1000 sq.ft.gfa	5/1000 sq.ft.gfa
Commercial Office	CO	3.5/1000 sq.ft.gfa	4.5/1000 sq.ft.gfa
Community Business	CB	4/1000 sq.ft.gfa	5/1000 sq.ft.gfa
General Commerce	GC	*	
City Center	CC		
Convenience Commercial		3.5/1000 sq.ft.gfa	4.5/1000 sq.ft.gfa
Office:			
Urban		2.0/1000 sq.ft.gfa.	3.0/1000 sq.ft.gfa
Campus		2.5/1000 sq.ft.gfa.	3.5/1000 sq.ft.gfa
Pedestrian			2.0/1000 sq.ft.gfa
Business Park	BP	2.0/1000 sq. ft. gfa	3/1000 sq. ft.gfa**
Light Industry	LI	2.0/1000 sq. ft. gfa	3/1000 sq. ft.gfa**
Heavy Industry	HI	2.0/1000 sq. ft. gfa	3/1000 sq. ft.gfa**
Planned Unit Developments	PD	2.0/1000 sq. ft. gfa	3/1000 sq. ft.gfa**

Notes: du - Dwelling Unit
gfa - Gross Floor Area
NS - No Specification
* - The number of spaces must be adequate to accommodate the peak shift as determined by the
** - The Technical Committee may consider parking at a ratio as low as 1.5/1000 if a covenant is recorded with the property which limits the uses to warehouse uses and/or limits the number of employees permitted in a building or project. Parking up to a ratio of 3.5/1000 may be considered if the employer/building owner can demonstrate the need for it and adopts a parking/traffic mitigation program.
Code Administrator after considering the probable number of employees, etc.
- Bedroom shall include all rooms that can be used as permanent sleeping quarters.

Amend Section 20C.20.140(10) REQUIREMENTS FOR OUTDOOR STORAGE as noted on the enclosed table

A Part of Subsection 20C.20.140(10) Requirements for Outdoor Storage				
ADD ZONE	PERMITTED STORAGE	REQUIREMENTS	AREA	SCREENING
<p>CITY CENTER CC exempt Design Area 4 REMOVE</p>	Public Street Furniture Sidewalk Restaurants Seasonal Items Special Public Event			
CB	Bulk Storage of more than 3 days with approval by Technical Committee except vehicle storage	Maximum height and width of four (4) feet	As Defined in 20C.20.140(35)	May be required by Technical Committee
	Non-Bulk Storage	Non-Bulk must be stored less than 24 hours		(non-bulk exempt)
<p>Design Area 4 REMOVE</p>	Public Street Furniture Sidewalk Restaurants Seasonal Items Special Public Events			
GC	Bulk Storage of more than 3 days with approval by Technical Committee including vehicle storage	Maximum Height ten (10) feet		Required as specified in 20C.20.140(20) & from streets & parks
	Non-Bulk Storage	Non-Bulk must be stored less than 24 hours		(non-bulk exempt)
A, BP, LI, and HI	All types	Maximum Height twenty (20) feet		Required as in 20C.20.140(20)
NB and R	Recreational vehicles as provided in Section 20C.20.150(25) Garden Materials & equipment			

A Part of Subsection 20C.20.230(30) SIGN REQUIREMENTS PER ZONING DISTRICT

SIGNS	ZONING DISTRICTS		
	GC; CB; CC - OFFICE, CONVENIENCE COMMERCIAL	CC - PEDESTRIAN PO; NB; CO; LI; HI; BP; R20; R30; EH-DESIGN DISTRICTS B & C	EH DESIGN DISTRICT A A-G-RE-R1-12 Non Residential
NUMBER OF SIGNS			
MAXIMUM NUMBER OF FREESTANDING OR PROJECTING SIGNS PER STREET FRONTAGE PER ESTABLISHMENT	1	1	1
FREESTANDING SIGNS			
Minimum Setback (feet)	5	5	10
Maximum Height (feet)	a height equal to the sign setback up to 10 feet	a height equal to the sign setback up to 10 feet	10
Maximum Size per Sign Face (Square Feet)	The smaller of 1% of the average gross floor area or 1 sq. foot per 4 feet of street frontage where the sign is to be placed up to a maximum of 75 sq. feet; but in any event, 25 sq. feet is permitted	The smaller of 1% of the average gross floor area or 1 sq. foot per 4 feet of street frontage where the sign is to be placed up to a maximum of 50 sq. feet; but in any event, 25 sq. feet is permitted	25
Maximum number of Sign Faces	4	4	2
WALL SIGNS			
Maximum Area (Square Feet)	The larger of 15% of the facade to which attached or 60 sq. feet up to a maximum of 300 sq. feet	The larger of 15% of the facade to which attached or 30 sq. feet up to a maximum of 100 sq. feet	60
Maximum Height (Feet)	Top of the wall or facade to which attached	Top of the wall or facade to which attached	20
PROJECTING SIGNS			
Maximum Area per Sign Face (Square Feet)	15	15	25
Maximum Area Total All Sign Faces (Square Feet)	30	30	50
Maximum Height (Feet)	Top of the wall or facade to which attached	Top of the wall or facade to which attached	20

Note: Individual Businesses In Multiple Building Complexes Not Permitted to have Freestanding Signs

Section 16. Section 20C.30.050 of the Redmond Municipal Code and Community Development Guide which currently appears as a section title only followed by the word "Reserved", is hereby amended by adding the following text to said Section:

20C.30.050(05) Purpose/Scope – The purpose of this section is to create guidelines for the development of the City Center Design District that is attractive in appearance, functionally integrated and promotes pedestrian and recreational activities.

Each Design Area is related to the other through circulation, building scale and open space relationships. The design criteria for the Design Areas are intended to identify general objectives and specific guidelines which will implement the objectives.

20C.30.050(10) Convenience Commercial Design Area

(a) Design Objective

1. Create a lower-scale suburban commercial area oriented to safe automobile access.
2. Provide visibility and access from automobiles while maintaining adequate landscaping and screening.
3. Develop features such as signs and artwork that identify the entrance to the City Center and provide a unifying visual theme for the area.

(b) Building Orientation and Access

1. Buildings are not required to be sited on the linkage system, but should provide safe and accessible pedestrian connections to it.
2. Service areas and rear entrances should not be oriented to public areas.
3. Orient buildings toward the streets and locate parking on the side or rear whenever possible.

(c) Building and Site Design

1. Develop sign programs to coordinate and consolidate signs in order to increase visibility from automobiles.
2. Minimize elements in signs such as too brilliant colors or other features that would distract from the entry feature.

3. Landscaping on streets should be simplified to allow adequate visibility from automobiles to businesses.
4. Parking lots should be landscaped to screen parking and provide visual relief from large asphalt surfaces.
5. Pedestrian access from parking areas should be safe and clearly defined.
6. Landscaped medians should be provided where access and traffic movements allow.
7. Conflict between pedestrians and automobiles should be avoided by minimizing vehicle crossings of pedestrian accessways.

20C.30.050(15) Office Design Area

(a) Design Objectives

1. The Office Design Area is intended to provide areas of greater structural density adjacent to the retail core to promote high levels of economic and pedestrian activity. These areas will provide transition from the auto-oriented convenience areas to the pedestrian core.
2. Density and building mass are increased in the Urban Office Design Area nearest the Center of the City Center District to encourage development of office-type structures of larger scale and size. Structured parking is required as heights increase over four stories to reduce the impact of on-site parking and allow greater open space at the ground level. Retail uses are also required at ground floor levels to encourage pedestrian activity.
3. Heights and density are moderated in peripheral areas of the district to promote a more suburban, campus-like setting adjacent to residential and convenience shopping areas.
4. Leary Way -- Leary Way between the Sammamish River and the Burlington Northern railroad tracks should remain as a "green gateway" to the City of Redmond. Setbacks from Leary Way should be adequate to preserve enough trees on the west side of the road to preserve the "green gateway" image.

The setback from the property line on this portion of Leary Way shall be at least 50 feet. No parking shall be allowed within the setback. No clearing, grading permit or site plan approval shall be granted until the approval authority determines that the existing trees will be preserved to the maximum extent possible within the setback.

Signage for traffic control, directional information, and business identification should be kept at a minimum in both size and number.

(b) Building Orientation and Access

1. Buildings should abut the linkage system on at least one side. Buildings on 164th Avenue N.E. or Redmond Way should orient to the linkage system on those streets.
2. Vehicle access should be designed to minimize interaction of vehicles with pedestrians particularly in relation to the linkage system.
3. Parking structures should not front on the linkage system.
4. Driveway openings to arterials should be minimized.

(c) Building and Site Design

1. Buildings should be sited in a manner compatible with adjacent buildings and the streetscape. Consideration should be given to size and height relationships with adjacent buildings.
2. Open space and landscaping should be coordinated and linked wherever possible, particularly in relation to public areas and the linkage system.
3. Landscaping should be designed with consideration of shade and sun; plazas and outdoor areas should be oriented to sunlight.
4. Outdoor and ground floor areas should be designed to encourage outdoor activities such as vendors, art displays, seating areas, outdoor cafes, abutting retail activities and other features of interest to pedestrians.

5. Taller buildings should have adequate separation to maintain a sense of openness, adequate light, and views.
6. Buildings should be constructed of materials that minimize light reflection and glare.
7. Facades should be divided into increments through the use of architectural features such as bay windows, offsets, recesses and other devices which break or minimize scale.
8. Rooftops should incorporate features which soften rectilinear forms and effectively screen mechanical equipment from view.
9. Buildings should be designed to provide for weather and wind protection at the ground level.
10. Encourage varieties of shapes, angles and reliefs in the upper stories of structures over four stories.

30.050(20) Pedestrian Design Area

(a) Design Objectives

1. The Pedestrian Design Area should be designed to provide safe and convenient access for pedestrians to retail and service activities.
2. Buildings, landscaping and signs should be scaled to pedestrians visually and functionally.
3. Pedestrians should be encouraged to move freely between buildings and into nearby Design Areas.
4. Landscaping should be appropriate to an urban environment and generally be more formal and compact in nature.

(b) Building Orientation

1. Buildings should abut the linkage system particularly on Redmond Way and Cleveland Streets.
2. Building entrances should face on the linkage system and be easily identifiable by pedestrians.

(c) Building and Site Design

1. Building design should be harmonious with adjacent Design Areas.

2. Landscaping should not overpower pedestrians or block views of crossings and intersections. Linkage system planting and design should provide landscaping bulbs or islands to decrease length of crossings and encourage pedestrian movement across streets.
3. Outdoor space should be designed to provide amenities for pedestrians such as seating and generally integrate business activities with pedestrian activity on the street.
4. Windows should be provided on the street level rather than blank walls to encourage a visual and economic link between the business and passing pedestrians.

30.050(25) Pedestrian Design Area (Old Town)

(a) Design Concept

1. "Historic Character" in the Old Town section of the Pedestrian Design Area refers to the wide variety of architectural styles which are representative of Redmond's history from the late 19th century through early 20th century, and which incorporate building materials and architectural treatments typically used during that period. Within the design concept of "historic character" one specific period or style of architecture is not dominant or thematic.
2. The design concept of "historic character" also encompasses the primary orientation of the built environment to the human or pedestrian scale. Historically, this was accomplished by the use of moderate building heights (2 - 3 stories), minimal setbacks from the pedestrian right-of-way and ground floor accessibility.

(b) General Criteria

1. Repair, renovation and new construction should retain the character of Old Town. This may be achieved by assuring that development meets the general requirements, and fits the special characteristics of Old Town.

2. New buildings must relate in material, scale and form. Contemporary designs which are harmonious with the surrounding environment in Old Town in terms of scale, materials, and color may be acceptable.
3. Buildings and facades should be brick, stone or wood. In all design there should be emphasis upon the quality of detail and special form in: window treatments, columns, eaves, cornices, lighting, signing, etc. Building facades should have a greater proportion of voids (windows) than solids (blank walls) on pedestrian levels.
4. Buildings and the spaces between should relate easily and openly to the external public areas.
5. The scale of all structures in relationship to other structures and spaces is important. The scale should be two to three stories. Some variation in heights contributes to the variety and complexity of the environmental experience, and is encouraged.
6. The development of ground level view points as well as public balcony and roof spaces which take advantage of solar access and views is encouraged.

(c) Physical Features

1. Permanent Building Elements
 - a. Buildings should be a backdrop to the Old Town activities. New or physical change will be evaluated for the effect on Old Town. The integrity of a building as an individual structure or as part of a series of buildings should be respected.
 - b. The main architectural elements should not be altered or disguised. Buildings as a whole should be seen as significant architectural elements.

- c. Exterior building surfaces above the first level and the primary architectural elements below the marquee such as columns, arches, rails are important elements to be considered in the building design and entry areas.
- d. The exteriors of significant historic buildings should be carefully restored and maintained. Other buildings should be related to the historic ones in terms of scale, proportion of openings, materials and color.

2. Pedestrian/Customer Elements

- a. Awnings - sunshades - canopies
Awnings or sunshades should be harmonious to the design of the building to which attached, and should be in keeping with the historic character of the area.
- b. Trees, plants and flowers
The use of potted plants and flowers as well as street trees are encouraged, but should not impede pedestrian traffic.
- c. Street lighting
Street lighting should relate in scale to the pedestrian characters of the area. The design of the light standards and lumineres should enhance an historic theme.
- d. Street Furniture
Public seating, trash receptacles and informational/directional kiosks should be of uniform design and be provided throughout Old Town.
- e. Sidewalks
Paving of sidewalks and other pedestrian rights-of-way should be safe and constructed of a uniform material that is compatible with the historic character. The private use of sidewalk rights-of-way areas may be appropriate for seasonal cafe seating or special displays.

3. Merchant Related Elements

a. Store fronts

1. Store fronts should be visually open wherever practical. Stores should use enough glass so that the activity inside the store is obvious to the passerby. In all cases merchandise should be easily visible to pedestrians.
2. Store fronts should not depart from the character of the building facade of which they are a part.
3. Store fronts should be brick, wood or stone where glass is not used. Care should be taken to avoid clashing colors on individual buildings and between adjacent buildings.

b. Signs


1. Signs will be evaluated according to the overall impact, size, shape, texture, lettering style, method of attachment, color, and lighting in relation to use, the building and street where the sign will be located, and the relation of the sign to other signs and other buildings in the vicinity. Adherence to or enhancement of sight lines both parallel and perpendicular to the sidewalks will be considered. The primary reference will be to the average pedestrian's eye-level view, although views into or down the street from adjacent buildings should be an integral feature of any review.
2. Signs should be simple, clear and direct. Generally, single-faced, flat-surfaced, painted signs are preferred. Extruded aluminum or plastic signs may not be appropriate. Careful use of neon may be acceptable. One of the many standard lettering styles should be used.

3. Signs should not hide or obscure the architectural elements of the building.
4. Exterior signs should be flat against the building, painted on it, or hung from the underside of the marquee, perpendicular to the sidewalk. Signs attached to the edge of the marquee should not extend above the marquee's upper edge.
5. Projecting, double-faced or three-dimensional signs may be allowed if they are integrated into the overall streetscape and do not destroy sight lines. Symbolic three-dimensional signs such as a shaving mug, barber pole, pawn shop symbol, or other symbols illustrating the product being sold on the premises, are acceptable provided they meet other sign guidelines. When possible, signs should reflect the character and the use within the structure.
6. Sign lighting should be subdued, incandescent, and front-lit from the exterior rather than back-lit of the fluorescent type.

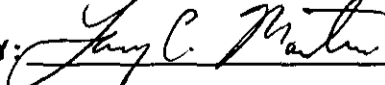
Section 17. This ordinance, being an exercise of a power specifically delegated to the City legislative body, is not subject to referendum, and shall take effect five (5) days after passage and publication.


MAYOR, DOREEN MARCHIONE

ATTEST/AUTHENTICATED:

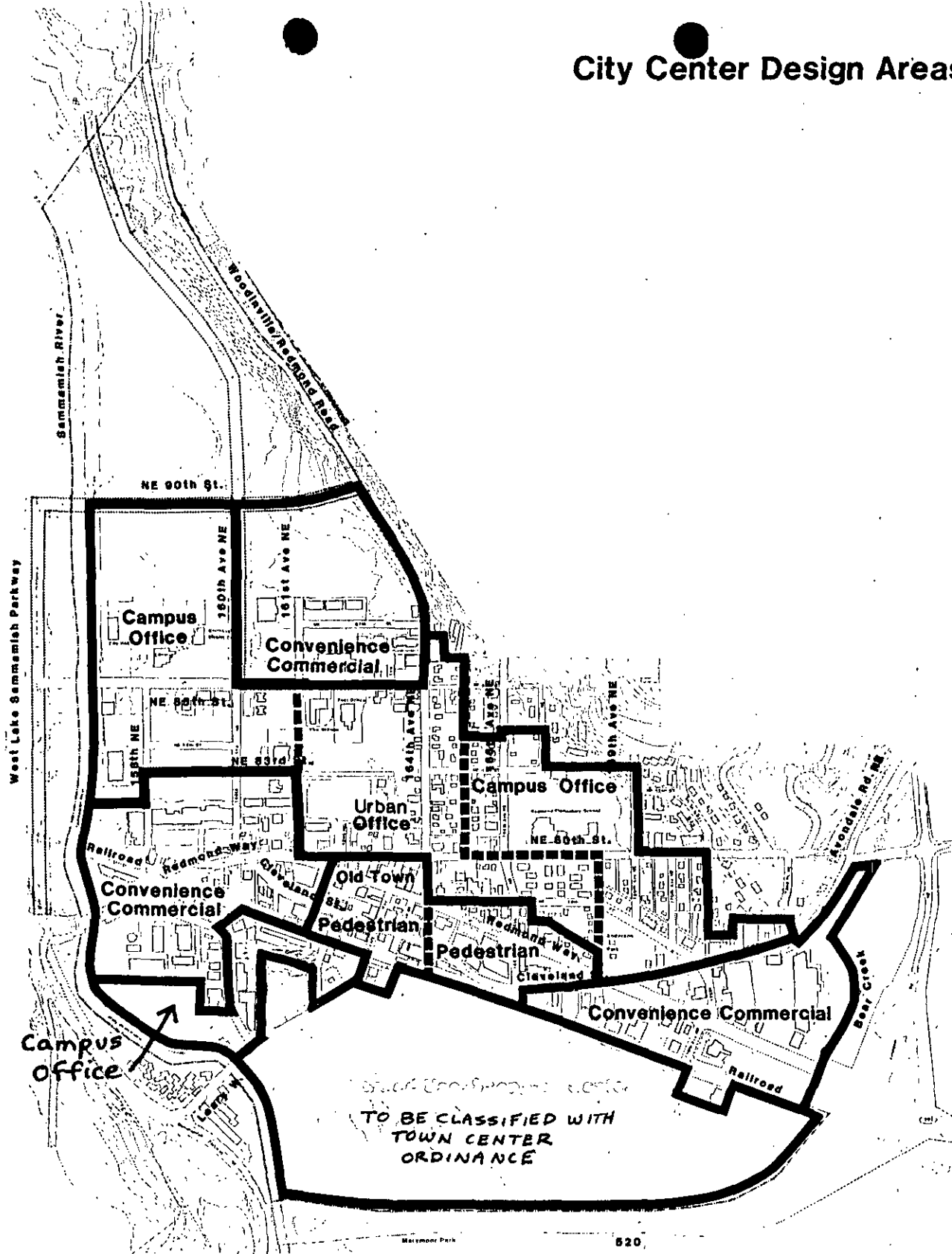

CITY CLERK, DORIS A. SCHAIBLE

APPROVED AS TO FORM:
OFFICE OF THE CITY ATTORNEY:

BY: 

FILED WITH THE CITY CLERK: March 31, 1988
PASSED BY THE CITY COUNCIL: April 19, 1988
PUBLISHED: May 8, 1988
EFFECTIVE DATE: May 13, 1988
ORDINANCE NO. 1415

City Center Design Areas



City Of Redmond, Washington
Department Of Planning and Community Development